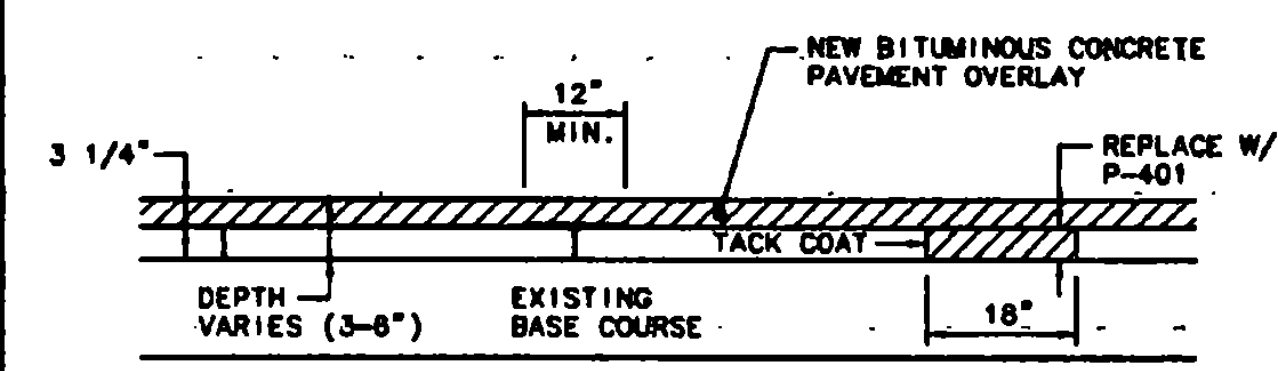


A PROPOSED TYPICAL SECTION R/W 1-19
STA 125+00 TO 142+00
SCALE = NONE



TYPE I CRACK REPAIR TYPE II CRACK REPAIR TYPE III CRACK REPAIR

G EXISTING RUNWAY PAVEMENT CRACK REPAIRS
SCALE = NONE

NOTES: 1. PRIOR TO PLACING NEW BITUMINOUS CONCRETE PAVEMENT OVERLAY, PAVEMENT CRACKS TO BE SEALED OR REPAIRED, AS FOLLOWS:

TYPE I CRACK REPAIR: CRACKS LESS THAN 3/4" IN WIDTH TO BE ROUTED AND SEALED WITH HOT POURED JOINT SEALING FILLER. PAVEMENT TO BE MADE UNDER ITEM 524.11 MOD. 1, TYPE I CRACK REPAIR (P-605).

TYPE II CRACK REPAIR: CRACKS BETWEEN 3/4" AND 1 1/4" IN WIDTH TO BE ROUTED SEALED WITH HOT POURED JOINT SEALING FILLER. PRIOR TO PLACING OVERLAY CRACKS BETWEEN 1/2" AND 1 1/4" SHALL BE COVERED WITH A PAVEMENT REINFORCEMENT MATERIAL SUCH AS "PAVE-PREP" TM OR APPROVED EQUAL. PAYMENT TO BE MADE UNDER ITEM 524.11 MOD. 2 (P-605), TYPE II CRACK REPAIR.

TYPE III CRACK REPAIR: CRACKS OVER 1 1/4" IN WIDTH TO BE CUT OUT. EXISTING BIT. CONC. PAVEMENT (8") ON EITHER SIDE OF THE CRACK TO BE REMOVED. BASE MATERIAL RE-COMPACTED, AND NEW BITUMINOUS CONCRETE PAVEMENT (CONFORMING TO ITEM 406.25 MOD. 1 P-401) REPLACED. PAYMENT TO BE MADE UNDER ITEM 524.11, MOD. 3 (P-605)

2. DETERMINATION OF USE OF TYPE I, TYPE II, OR TYPE III CRACK REPAIR WILL BE AS DIRECTED BY THE ENGINEER.

3. CRACK SURVEY IN DEC. 1986 REVEALED APPROXIMATELY 17,000 LF OF CRACKS WHICH NEEDED REPAIR. APPROXIMATELY 10% TYPE III, APPROXIMATELY 50% TYPE II AND APPROXIMATELY 40% TYPE I.

GENERAL NOTES

- SUBGRADE UNDER AREAS TO BE PAVED TO BE COMPACTED TO 100% DENSITY AS PER AASHTO T-180.
- BITUMINOUS TACK COAT (EMULSIFIED ASPHALT, RS-1) TO BE APPLIED BETWEEN LIFTS OF BITUMINOUS CONCRETE PAVEMENT AT A RATE OF 0.01 TO 0.03 GAL / SY WHEN DIRECTED BY THE ENGINEER. (NOT A SEPARATE PAY ITEM.)
- REMOVAL OF EXISTING MANHOLES AND DROP INLETS, TO BE PAID UNDER ITEM 203.16 SOLID ROCK EXCAVATION. (3 CY PER EACH UNIT REMOVED). EXISTING DRAINAGE LINES TO BE ABANDONED IN PLACE; PIPES TO BE PLUGGED EACH END WITH CONCRETE BEFORE BACKFILLING. PAYMENT TO BE SUBSIDIARY TO REMOVAL OF MANHOLES AND DROP INLETS.
- EXISTING BITUMINOUS PAVEMENT 80'-75' EACH SIDE OF CENTERLINE TO BE REMOVED. PAYMENT TO BE MADE UNDER ITEM 203.15, COMMON EXCAVATION. AREA TO BE GRADED, TOPSOILED AND SEED.
- ALL EXCAVATION TO BE PAID FOR UNDER ITEM 203.15, COMMON EXCAVATION OR 203.16 SOLID ROCK EXCAVATION.
- SEED, ITEM 651.15 TO BE APPLIED AS DIRECTED BY ENGINEER.

X WT	LBS / A	NAME	FUR %	GERM
3.33	2	CROWN VETCH	97	75
50.00	30	CREeping RED FESCUE	88	85
8.33	5	TIMOTHY	99	85
18.67	10	PERR. RYE GRASS	95	85
		(VAR. PENNIFINE)		
8.34	5	ALFALFA (VAR. SARANAC)	99	85
8.33	5	BIRDFOOT TREFOIL (VAR. EMPIRE)	98	85
5.00	3	HIGHLAND BENT GRASS	92	85
100.00	80			

THE SEED MIXTURE SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.

FERTILIZER, ITEM 651.18, FORMULA 10-20-10 TO BE USED WITH SEED, ITEM 651.15, APPLIED AT THE RATE IF 500 LBS / ACRE.

AGRICULTURAL LIMESTONE, ITEM 651.20, TO BE APPLIED AT RATE OF 2 TONS / ACRE OR AS DIRECTED BY ENGINEER.

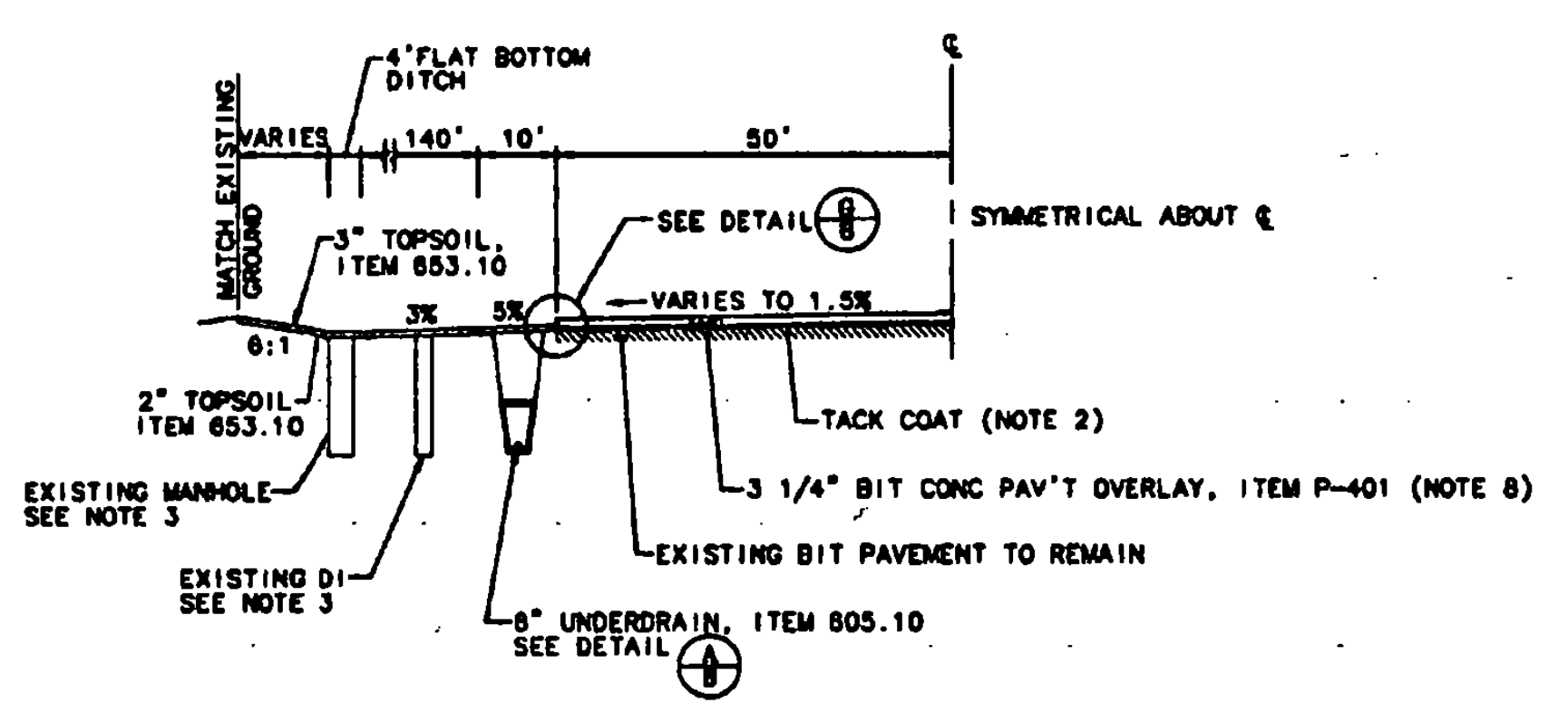
HAY MULCH, ITEM 651.25, TO BE PLACED ON EARTH SLOPES AT THE RATE OF 2 TONS / ACRE.

TOPSOIL, ITEM 651.35, TO BE USED WITH SEED, ITEM 651.15, AS DIRECTED BY THE ENGINEER.

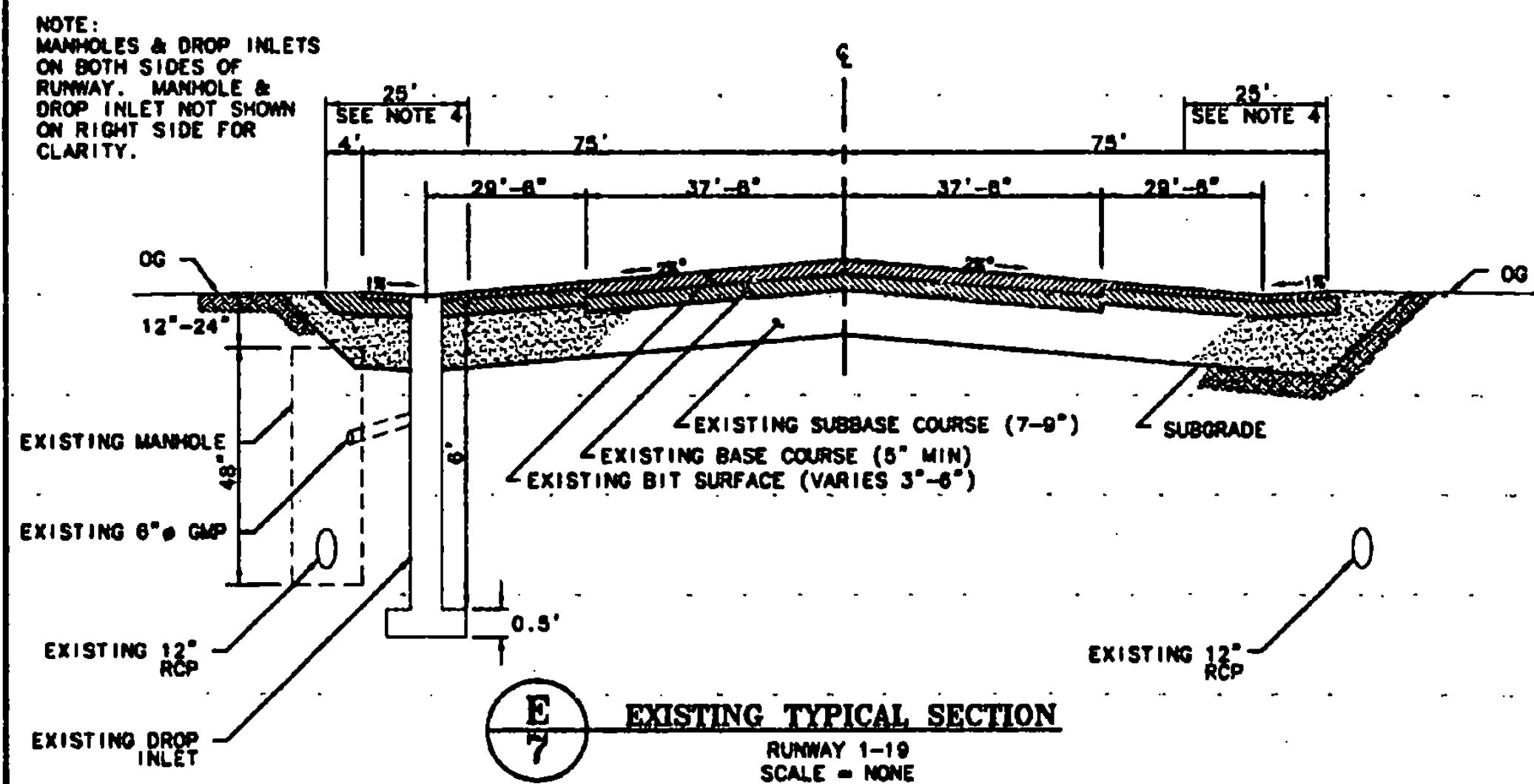
ALLOWABLE THICKNESS TOLERANCES:
SUBGRADE ± 1"
CONTROLLED MATERIAL ± 1"
PAVEMENT ± 3/16"
BASE COURSE ± 1/2"

7. BITUMINOUS CONCRETE PAVEMENT DESIGN BASED ON PERFORMANCE GRADED ASPHALT - USE PG 58-34 IN ACCORDANCE WITH AASHTO DESIGNATION MP1.

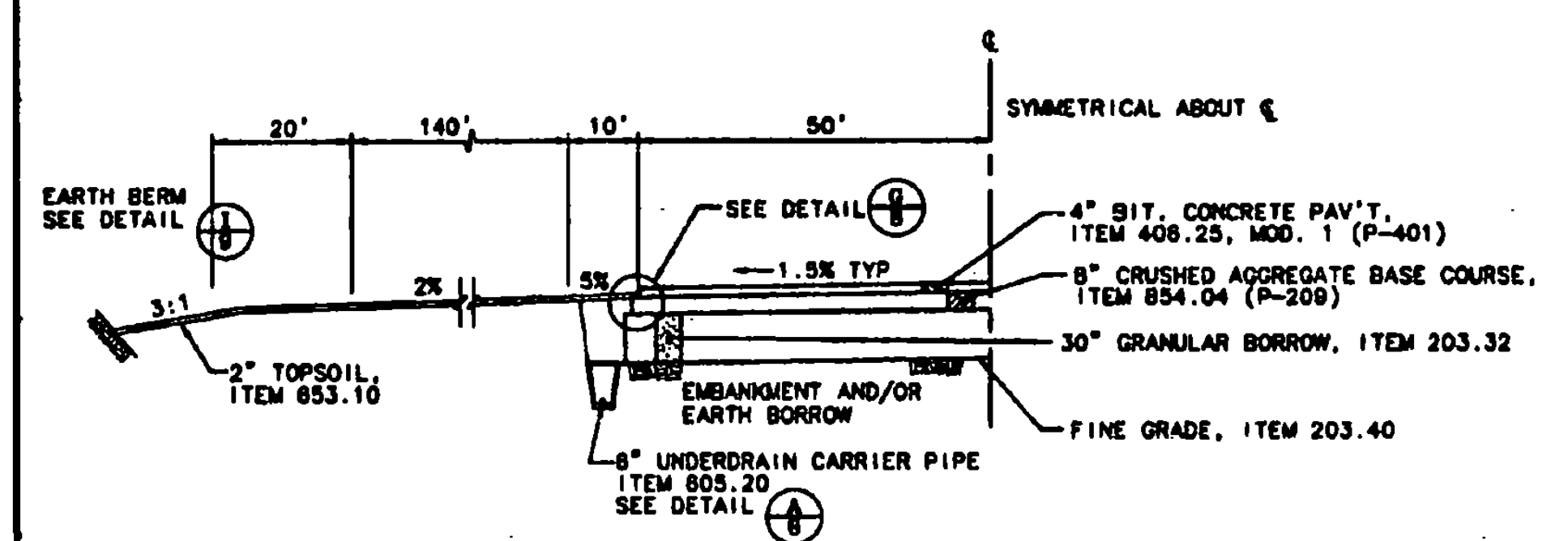
8. MINIMUM OVERLAY DEPTH IS 2 INCHES - MILL EXISTING PAVEMENT GRADED ASPHALT - WHEN NECESSARY TO ACHIEVE MINIMUM THICKNESS.



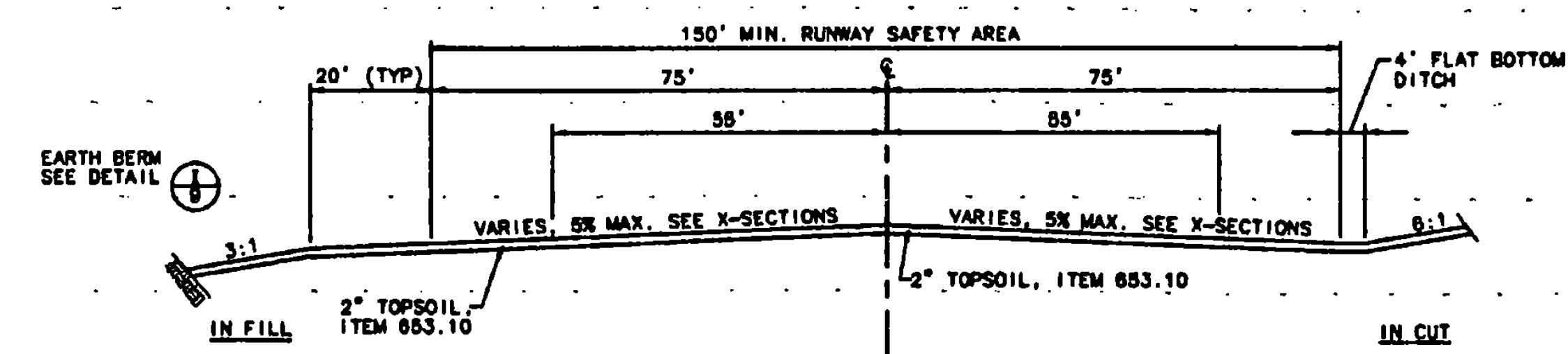
B PROPOSED TYPICAL SECTION R/W 1-19
STA 103+00 TO 124+50
SCALE = NONE



E EXISTING TYPICAL SECTION
RUNWAY 1-19
SCALE = NONE

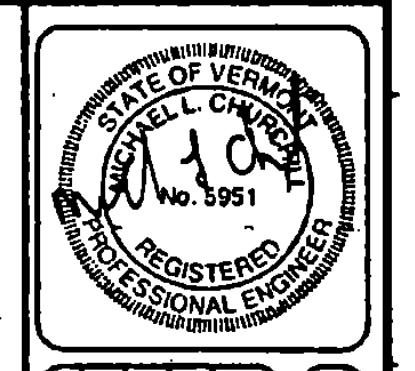


C PROPOSED TYPICAL SECTION R/W 1-19
STA 150+00 TO 153+00
SCALE = NONE



D RUNWAY SAFETY AREA BEYOND RUNWAY THRESHOLD - TYPICAL SECTIONS
STA 100+00 TO 103+00
STA 153+00 TO 156+00
SCALE = NONE

* WHERE SHOWN ON PLANS OR AS ORDERED BY THE ENGINEER



REV.	DATE	DESCRIPTION

Job No. _____

RUTLAND STATE AIRPORT
CLARENDON, VERMONT

TYPICAL SECTIONS & DETAILS

URS Greiner, Inc.
3 MARCUS BOULEVARD
ALBANY, NEW YORK

Designed by: C. D'Amico 2/87	Drawn by: M. Scallala 3/87	Checked by: C. D'Amico 9/87	Approved by: R. Greiner 9/87
Scale: HS = AS NOTED VS = AS NOTED			
Date: 8/8/87			
Sheet 7 of 88			
Sheet No. 7			